



Project Overview

The new 5.5-mile I-10 Calcasieu River Bridge Project stretches from Ryan Street in Lake Charles to the I-10/I-210 interchange in Westlake. The project replaces the aging bridge with a modern structure and includes a railroad overpass on Sampson Street. The overpass will eliminate vehicle backups caused by rail traffic, while the new bridge's six travel lanes and two auxiliary lanes will ensure safer and more efficient travel for the residents and businesses of Southwest Louisiana. The project is a public-private partnership (P3) and received significant funding from state and federal sources.

What is a P3?

A P3 bridge project is a public-private partnership (P3) that involves a public agency and a private entity working together to design, build, finance, and maintain a bridge. P3s are long-term projects that use a performance-based approach, with the private sector taking on a large share of the risk.

A P3 allows the state to access private capital for large infrastructure projects, avoiding upfront construction costs, freeing up public funds for other priorities while still gaining access to necessary infrastructure development. Projects are paid for over time. In this case, through a 50-year toll agreement.

Benefits of the P3 model for the new I-10 Calcasieu River Bridge Project

Access to private funding: P3s allow the DOTD to access private sector equity investment, which reduces the need for public funding.

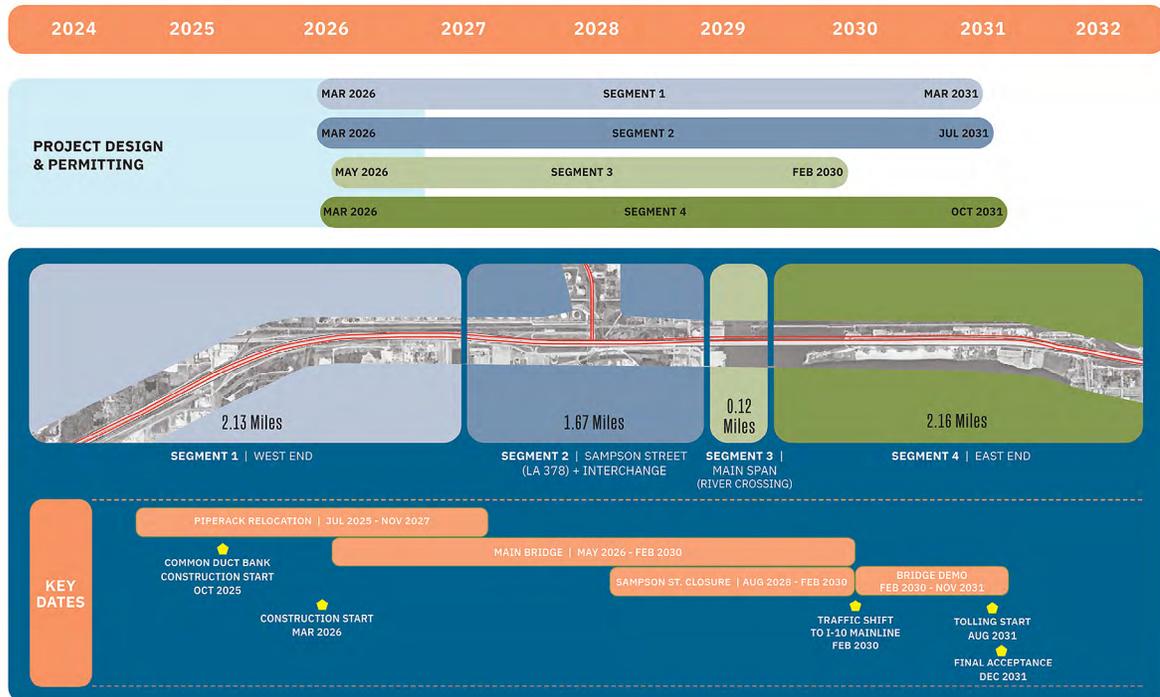
Efficiency: P3s use a single contracting instrument for financing, design, construction, maintenance, and operation, which is more efficient than administering multiple contracts.

Risk Sharing: Risks such as cost overruns and delays are shared between the government and the private sector, reducing the burden on taxpayers.





Project Timeline



Features of the New I-10 Calcasieu River Bridge Project

Safety: The new bridge will have three travel lanes plus full shoulders with auxiliary lanes in each direction, improving safety and traffic management during emergencies. The current bridge is the steepest in Louisiana with a grade elevation of 152 feet, 10 inches. Once the new bridge design is finalized, the finished grade elevation is expected to be between 75 and 95 feet.

Design: The bridge will be made of pre-cast, prestressed concrete girders and cast-in-place composite concrete decks. The lighting will mirror the shape of the original bridge’s trusses, and the pylons will have a metal truss pattern with sculptures of crossed pistols.

Electronic Tolling: A system of lights and cameras will capture GeauxPass information or read license plates for billing.

Age and Use of Current Bridge: This project replaces a bridge built in 1952 with a projected 50-year lifespan to accommodate a load of up to 37,000 vehicles per day. At more than 70 years old, the current bridge is currently being used by more than 90,000 vehicles per day.



Regional Benefits of the New I-10 Calcasieu River Bridge Project

Improved safety: Vehicles on the bridge and marine traffic will be able to travel more safely than the current aging bridge allows.

Improved traffic flow: Three lanes of I-10 in each direction through the project will move traffic smoothly through the region. The Sampson Street overpass will eliminate commuter and commercial vehicle backup currently caused by rail traffic.

Reduced travel disruptions: Currently the bridge requires frequent repairs and maintenance, which will be minimized with the new bridge.

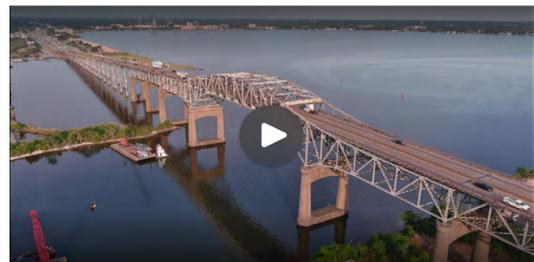
Economic growth: Enhanced infrastructure supports improved access, which provides more economic and growth opportunities within surrounding communities.

Visual Assets

**Click for link to hi res version*

[I-10 Eastbound - Sampson Street Overpass \(project video\)](#)

[I-10 Westbound \(project video\)](#)





MEDIA KIT

2026-2027

Common Duct Bank

The common duct bank serves as a centralized, highly secure underground housing system designed to safely consolidate and relocate utilities. By shifting key utilities into this single, fortified corridor, we are effectively clearing the canvas for the new 5.5-mile interstate alignment. It will also have the capacity for future utilities to be included.

This part of the project represents a significant upgrade to the region's infrastructure resilience. Housed safely underground, these vital utilities will be far better protected from severe weather events and environmental wear, while remaining safely accessible for future municipal maintenance.

Visual Assets

[Duct Bank Work Photos](#)

Operations & Maintenance Repairs

2025 O&M Repairs	
Element	Number of Repairs
Roadway	43
Drainage	62
Structures	1
Pavement Markings	46
Guardrails	39
Traffic Lights and Signs	15
Roadway Lighting	77
Fencing	5
Earth Works	8
Debris Removal	109
Sweeping	40
Litter Removal	279

Recent Media Coverage

[New I-10 Bridge Expected to Open to Traffic in February 2030](#)

[A Bridge to Progress: New Videos Highlight I-10 Calcasieu River Bridge Replacement Project](#)

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